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**Anthony Lee Washington III**  
**2025.03.10 17:53:13-05'00'**

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HDR ENGINEERING, INC.  
120 BRENTWOOD COMMONS WAY  
SUITE 525  
BRENTWOOD, TN 37027  
ANTHONY L. WASHINGTON III P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND	
STANDARD TRAFFIC DESIGN DRAWINGS .....	1A
PROJECT COMMITMENTS .....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES .....	2F
UTILITY NOTES, AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	4

YEAR	PROJECT NO.	SHEET NO.
2025	NH/HSIP-15(232)	ROADWAY-SIGN1

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SIGNATURE  
SHEET





Index Of Sheets  
SEE SHEET NO. 1A

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

HARDIN COUNTY

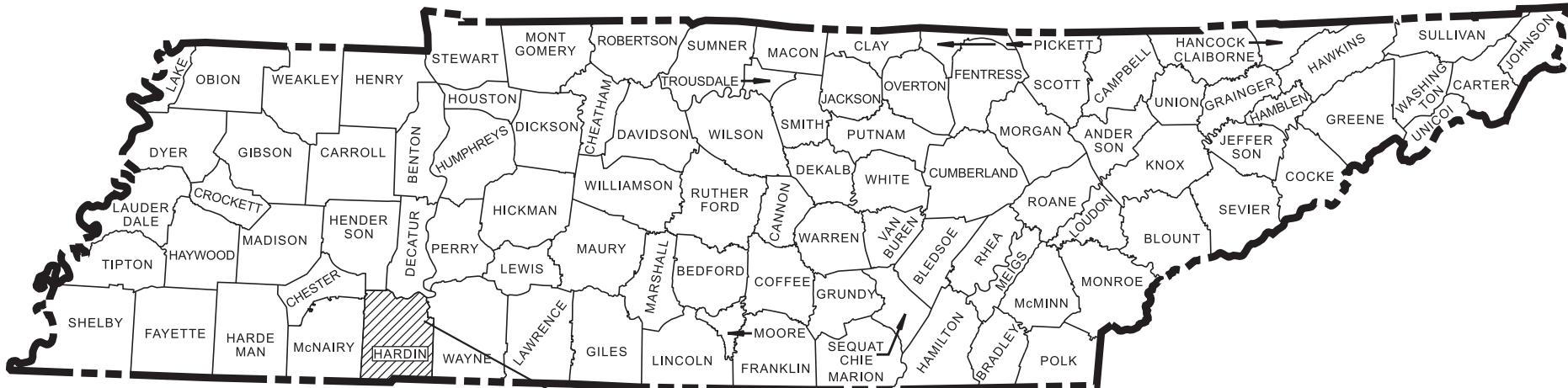
S.R. 15  
FROM: L.M. 21.15 (INDIAN CREEK BRIDGE)  
TO: L.M. 23.62 (WAYNE COUNTY LINE)

RESURFACE  
MILL, 411D, 411E (SHOULDERS), GUARDRAIL, & PAVEMENT MARKINGS

STATE HIGHWAY NO. 15 F.A.H.S. NO. US-64

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH/HSIP-15(232)	
STATE PROJ. NO.	36S015-F8-006	
STATE PROJ. NO.	36S015-F3-006	
STATE PROJ. NO.	36S015-M3-005	



PROJECT LOCATION  
BRIDGE ID. # 36SR0150025 36SR0150026 36SR0150027 36SR0150029

36S015-F8-006  
36S015-F3-006  
END PROJECT NO. NH/HSIP-15(232) RESURFACE  
L.M. 23.62 (WAYNE COUNTY LINE)

BRIDGE DECK REPAIR PROJECT NO. 36S015-M3-005  
SR-15 L.M. 21.18L  
SR-15 L.M. 21.18R  
SR-15 L.M. 22.34L  
SR-15 L.M. 22.34R

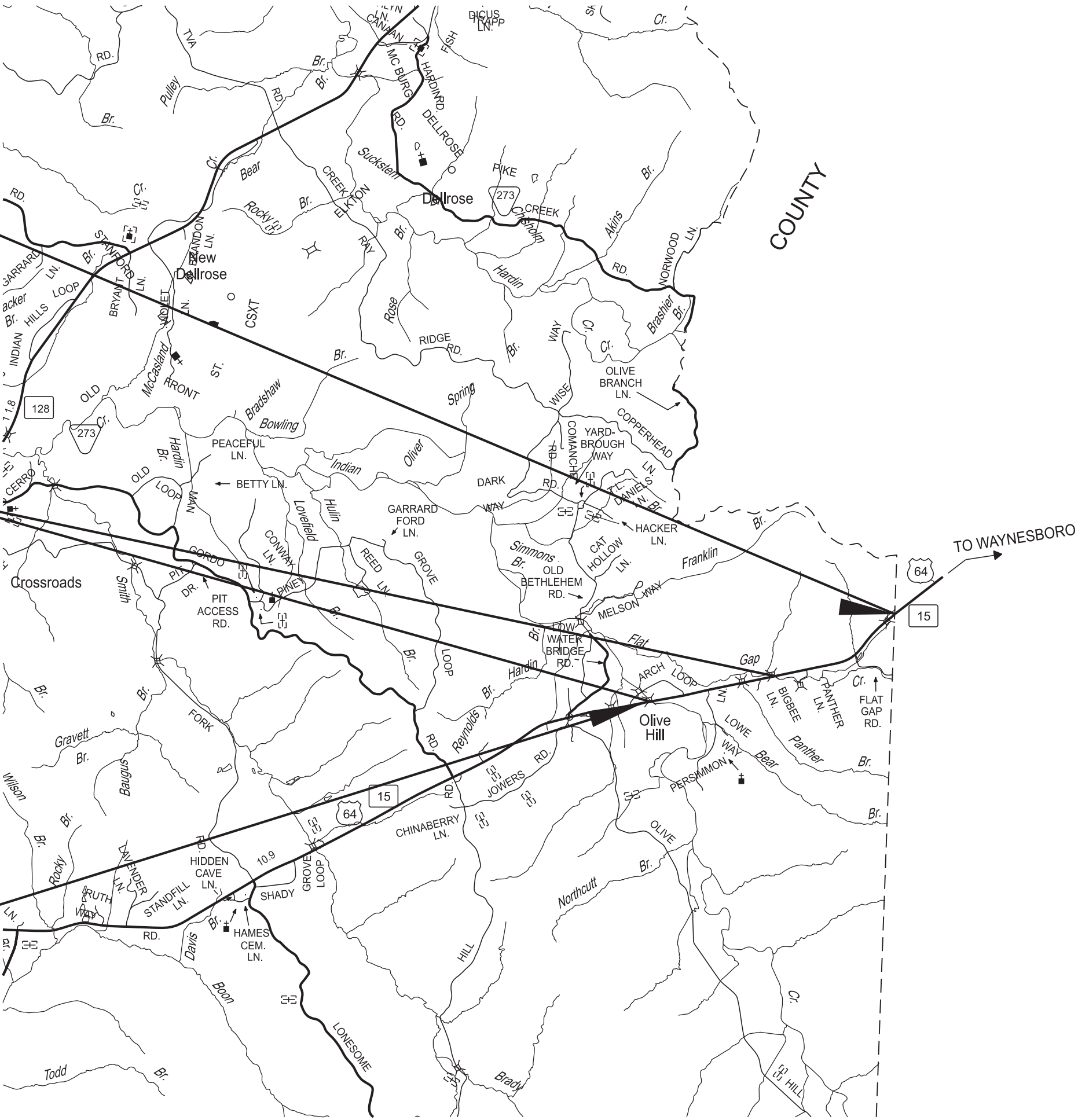
36S015-F8-006  
36S015-F3-006  
BEGIN PROJECT NO. NH/HSIP-15(232) RESURFACE  
L.M. 21.15 (INDIAN CREEK BRIDGE)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

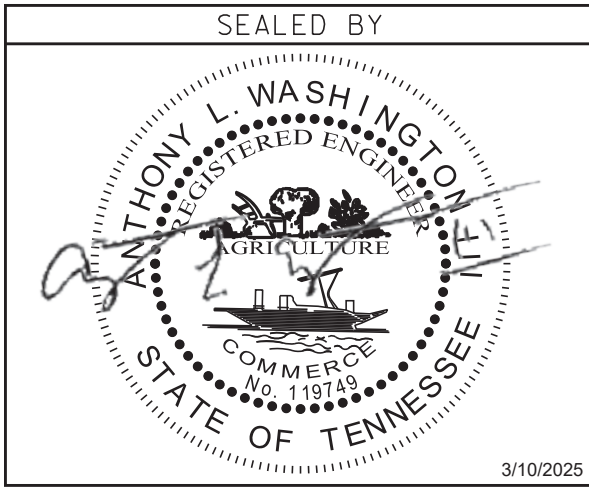
TDOT PROJECT MANAGER : LYNN EVANS, P.E., REG. 4  
DESIGNED BY : HDR ENGINEERING, INC.  
DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.  
P.E. NO. 98043-4283-04  
PIN NO. 133162.00



SCALE: 1" = 5280'

PROJECT LENGTH 2.47 MILES  
TOTAL LANE MILES RESURFACED 9.88 MILES

NO EXCLUSIONS



APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, COMMISSIONER

TRAFFIC COUNTER,  
EMBEDDED DETECTION LOOPS &  
AUTOMATIC TRAFFIC READERS

STATION LOCATION	LOG MILE
TC STATION 31	21.35

TRAFFIC DATA	
ADT (2025)	3,355
POSTED SPEED LIMITS	
L.M. 21.15 TO L.M. 23.62 55 MPH	

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION  
APPROVED: DIVISION ADMINISTRATOR DATE



ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
.....	ROADWAY-SIGN2
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND	
STANDARD TRAFFIC DESIGN DRAWINGS .....	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES .....	2F
UTILITY NOTES, AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	4
BRIDGE PLANS.....	B-1
NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS.	

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
SAFETY DESIGN AND GUARDRAILS		
S-GR31-1	06-15-21	GUARDRAIL DETAILS
S-GR31-1A	06-28-19	GUARDRAIL AND BLOCK-OUT DETAILS
S-GR31-1B		GUARDRAIL FASTENING HARDWARE
S-GR31-1C	07-07-23	GUARDRAIL GENERAL NOTES AND POST DETAILS
S-GR31-1D	03-01-23	GUARDRAIL POST PLACEMENT IN ROCK
S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRA-3	01-09-24	TYPE 13 GUARDRAIL ANCHOR


STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
DESIGN – TRAFFIC CONTROL		
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-15A	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED RURAL ROUTES
T-M-16	01-24-25	RUMBLE STRIPE INSTALLATION LAYOUT
T-M-16A	01-24-25	RUMBLE STRIPE DETAILS FOR EDGE OF PAVEMENT AND CENTERLINE
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-15(232)	1A

REV. 04-02-25: ADDED SHEET ROADWAY-SIGN2 TO INDEX.

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4/2/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

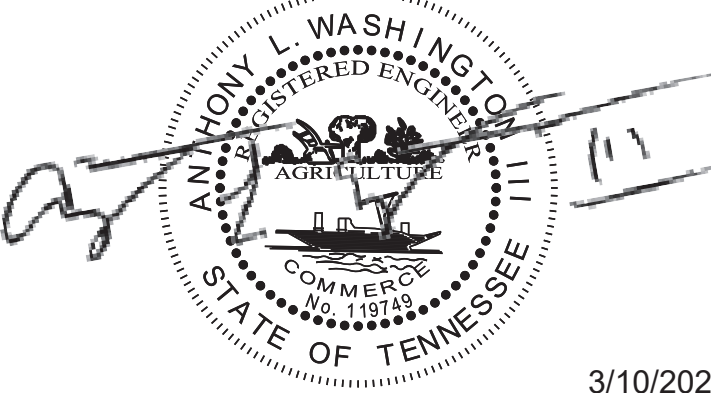
ROADWAY INDEX,  
STANDARD ROADWAY  
DRAWINGS, AND  
STANDARD TRAFFIC  
DESIGN DRAWINGS



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-15(232)	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
<div>EDHZ001</div>	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON THE FOLLOWING BRIDGES AND NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. BRIDGE NO. 36SR0150025 SR-15 EB OVER INDIAN CREEK LM 21.18 (36-SR015-21.18R) BRIDGE NO. 36SR0150026 SR-15 WB OVER INDIAN CREEK LM 21.18 (36-SR015-21.18L) BRIDGE NO. 36SR0150029 SR-15 EB OVER FLAT GAP CREEK LM 22.35 (36-SR015-22.35R) BRIDGE NO. 36SR0150030 SR-15 WB OVER FLAT GAP CREEK LM 22.35 (36-SR015-22.35L)	BRIDGE NO. 36SR0150025 SR-15 EB OVER INDIAN CREEK LM 21.18 (36-SR015-21.18R) BRIDGE NO. 36SR0150026 SR-15 WB OVER INDIAN CREEK LM 21.18 (36-SR015-21.18L) BRIDGE NO. 36SR0150029 SR-15 EB OVER FLAT GAP CREEK LM 22.35 (36-SR015-22.35R) BRIDGE NO. 36SR0150030 SR-15 WB OVER FLAT GAP CREEK LM 22.35 (36-SR015-22.35L)
<div>EDHZ002</div>	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGE NO. 36SR0150025 SR-15 EB OVER INDIAN CREEK LM 21.18 (36-SR015-21.18R) BRIDGE NO. 36SR0150026 SR-15 WB OVER INDIAN CREEK LM 21.18 (36-SR015-21.18L) BRIDGE NO. 36SR0150029 SR-15 EB OVER FLAT GAP CREEK LM 22.35 (36-SR015-22.35R) BRIDGE NO. 36SR0150030 SR-15 WB OVER FLAT GAP CREEK LM 22.35 (36-SR015-22.35L)

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3/10/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PROJECT  
COMMITMENTS

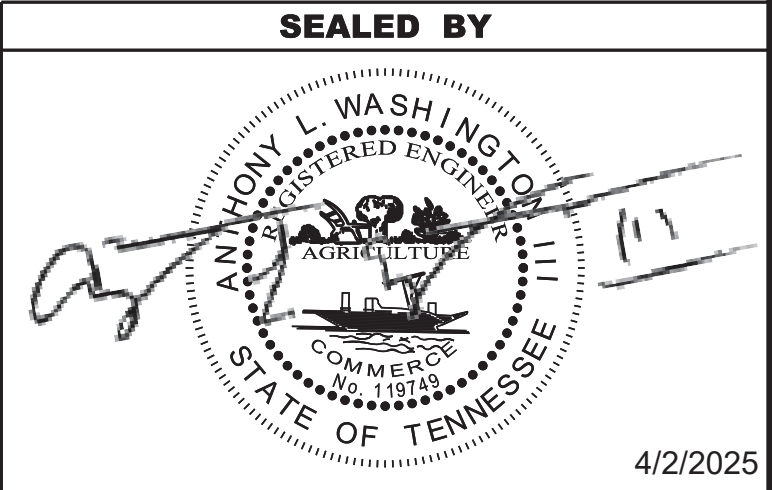


ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 36S015-F8-006	QUANTITY 36S015-F3-006	TOTAL QUANTITY
(1)	202-03.01 Removal Of Asphalt Pavement	S.Y.	2300		2300
	203-06 Water	M.G.	17		17
(2)(3)	208-01.05 Brooming & Degrassing Shoulders	L.M.	10		10
(4)	303-02 Mineral Aggregate, Type B Base, Grading (C or D)	TON	2188		2188
(5)(6)	307-02.01 Asphalt Concrete Mix (Pg70-22) (Bpmb-Hm) Grading A	TON	299		299
(5)(6)	307-02.08 Asphalt Concrete Mix (Pg70-22) (Bpmb-Hm) Grading B-M2	TON	113		113
(7)	403-02.01 Trackless Tack Coat	TON	48		48
(8)	411-01.21 Longitudinal Joint Sealant	L.M.	10.1		10.1
	411-02.11 Acs Mix(Pg70-22) Grading E Rdwy	TON	2017		2017
(9)(20)	411-02.15 Acs Mix(Pg70-22) Grading D Limestone Aggregate	TON	6035		6035
	411-12.02 Scoring Shoulders (Non-Continuous) (16In Width)	L.M.	4		4
	411-12.03 Scoring For Rumble Stripe (Non-Continuous) (8In Width)	L.M.	4.9		4.9
(10)	415-01.01 Cold Planing Bituminous Pavement	TON	7777		7777
(11)	705-02.10 Guardrail Transition 27In To 31In	EACH		8	8
(11)	705-06.10 Gr Terminaltrailing End (Type 13) Mash TI-3	EACH		4	4
(11)	705-06.20 Tangent Energy Absorbing Term Mash TI-3	EACH		4	4
(11)	706-01 Guardrail Removed	L.F.		240	240
(12)	712-01 Traffic Control	LS	1		1
	712-04.01 Flexible Drums (Channelizing)	EACH	125		125
	712-05.03 Warning Lights (Type C)	EACH	26		26
(13)(14)	712-06 Signs (Construction)	S.F.	1017		1017
	712-08.03 Arrow Board (Type C)	EACH	4		4
	716-01.22 Snowplowable Raised Pavment Markers (Mono-Dir)(1 Color)	EACH	10		10
	716-01.23 Snowplowable Raised Pavement Markers (Bi-Dir)(2 Color)	EACH	326		326
(15)	716-01.30 Removal Of Snowplowable Reflective Marker	EACH	336		336
(16)(17)(18)	716-02.05 Plastic Pavement Marking (Stop Line)	L.F.	59		59
(16)(17)	716-02.06 Plastic Pavement Marking (Turn Lane Arrow)	EACH	3		3
(16)(17)	716-02.12 Plastic Pavement Marking (8In Line)	L.M.	0.1		0.1
(19)	716-05.20 Painted Pavement Marking (6" Line)	L.M.	22.5		22.5
(16)(17)	716-13.02 Spray Thermo Pvmt Mrkng (60 Mil) (6In Line)	L.M.		11.1	11.1
(16)(17)	716-13.05 Spray Thermo Pvmt Mrkng (60 Mil) (6In Dotted Line)	L.F.		318	318
	717-01 Mobilization	LS	1		1

FOOTNOTES	
(1)	INCLUDES 1300 SY FOR "A" MIX AT 4-INCH DEPTH AND 1000 SY FOR "B-M2" AT 2-INCH DEPTH.
(2)	TO BE USED AS DIRECTED BY THE ENGINEER.
(3)	INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDER PRIOR TO WORK. SEE SHEET 2D, PAVEMENT, RESURFACING, NOTE 2 FOR MORE INFORMATION.
(4)	INCLUDES 880 TONS FOR BREAKOUT AREAS.
(5)	ITEM TO BE USED FOR BREAKOUT AREAS.
(6)	FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01.
(7)	INCLUDES 3 TONS FOR PRIVATE DRIVES, BUSINESS ENTRANCES, FIELD ENTRANCES, COUNTY ROADS, CITY STREETS, INTERSECTIONS, AND EXTRA WIDTH AREAS.
(8)	USE CRAFTCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO. TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER.
(9)	INCLUDES 362 TONS FOR PRIVATE DRIVES, BUSINESS ENTRANCES, FIELD ENTRANCES, COUNTY ROADS, CITY STREETS, INTERSECTIONS, AND EXTRA WIDTH AREAS. INCLUDES AN ADDITIONAL 200 TONS FOR SPOT LEVELING.
(10)	INCLUDES 359 TONS FOR PRIVATE DRIVES, BUSINESS ENTRANCES, FIELD ENTRANCES, COUNTY ROADS, CITY STREETS, INTERSECTIONS, AND EXTRA WIDTH AREAS.
(11)	SEE GUARDRAIL TABULATION BLOCK ON SHEET 2F.
(12)	THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
(13)	SEE CONSTRUCTION SIGN TABULATION BLOCK ON SHEET 2F.
(14)	THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION).
(15)	IF, DURING REMOVAL, DAMAGE OCCURS TO THE EXISTING ASPHALT BELOW THE SCHEDULED MILLING DEPTH, THE CONTRACTOR SHALL PATCH THE DAMAGE, AT THEIR OWN EXPENSE, AS DIRECTED BY THE ENGINEER.
(16)	ITEM TO BE USED AS PERMANENT MARKING ONLY.
(17)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(18)	ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
(19)	ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY.
(20)	THE COARSE AGGREGATE USED IN THE ASPHALT MIXTURE SHALL BE AN APPROVED LIMESTONE SURFACE AGGREGATE MEETING THE REQUIREMENTS OF 903.24. NO GRAVELS OR FRACTURED MATERIALS FROM GRAVELS SHALL BE ALLOWED IN THIS MIXTURE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSSIP-15(232)	2

REV. 04-02-25: DELETED ITEM 411-02.10;  
ADDED ITEM 411-02.15; ADDED FOOTNOTE 20 TO ITEM 411-02.15



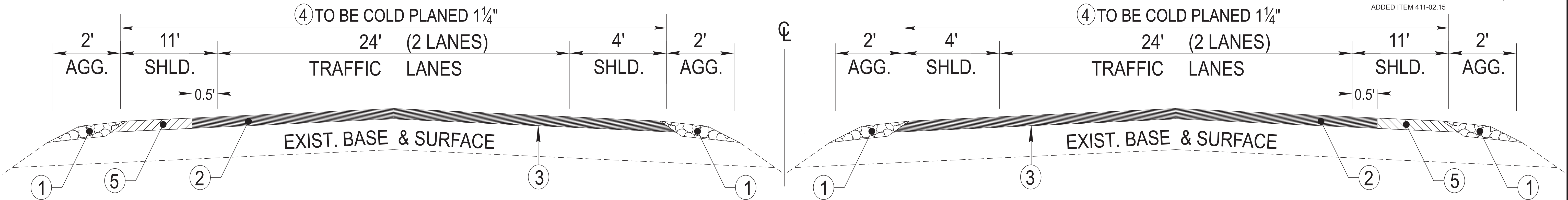
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED  
ROADWAY  
QUANTITIES

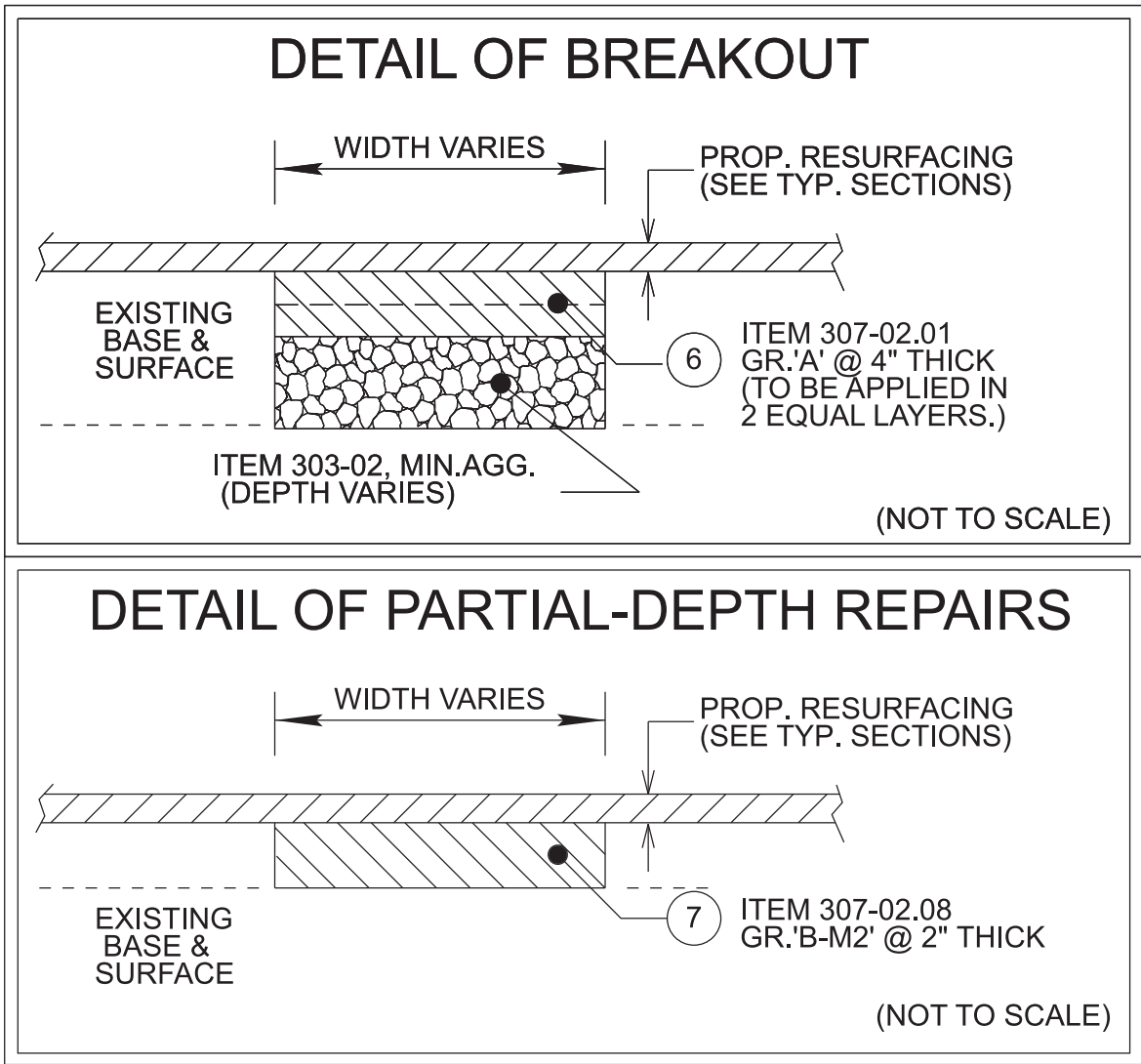


TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSSIP-15(232)	2B

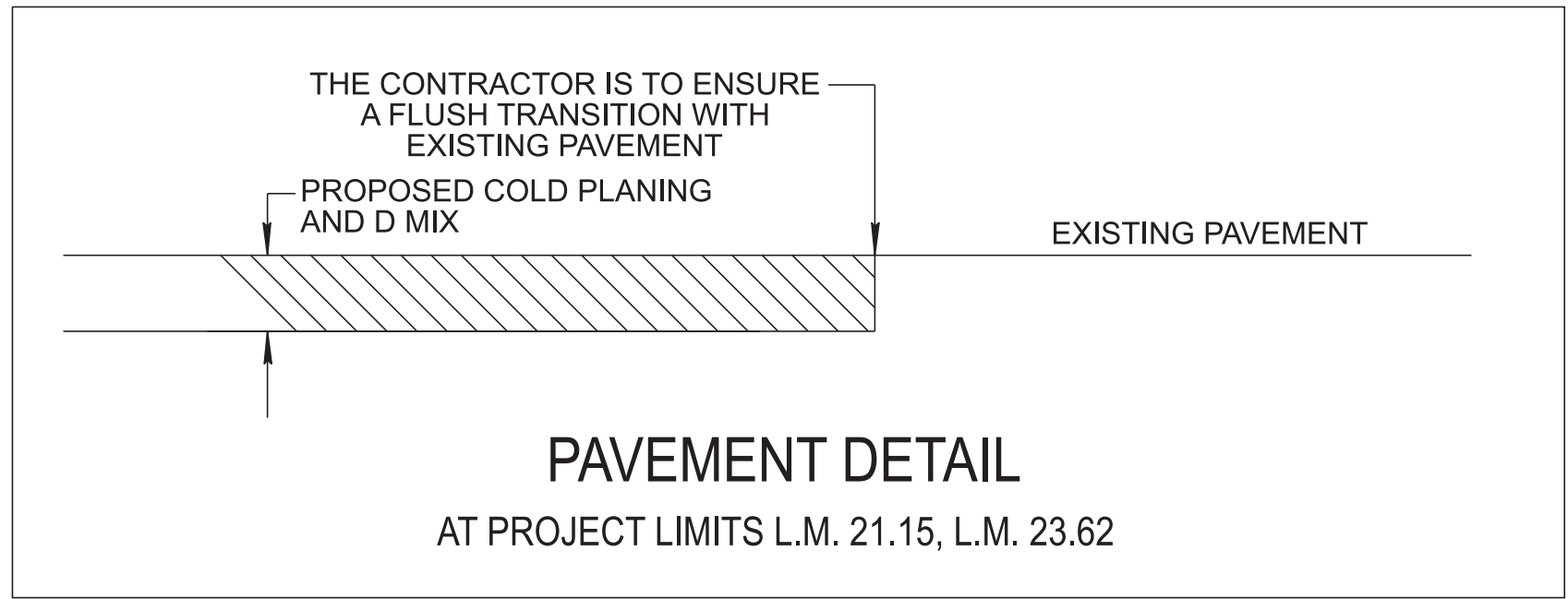
REV. 04-02-25: DELETED ITEM 411-02.10;  
ADDED ITEM 411-02.15



SR 15  
TYPICAL SECTION  
L.M. 21.15 TO L.M. 23.62



PROPOSED PAVEMENT SCHEDULE	
①	MINERAL AGGREGATE BASE @ 2.00"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)
②	ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.) ITEM 411-02.15 ACS MIX (PG70-22) GRADING "D" LIMESTONE AGGREGATE
③	TACK COAT ITEM 403-02.01 TRACKLESS TACK COAT (TO) (TON) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
④	COLD PLANING @ 1.25"± THICK (APPROX. 131.25 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
⑤	ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.) ITEM 411-02.11 ACS MIX (PG70-22) GRADING "E" ROADWAY
⑥	BITUMINOUS COURSE (BLACK BASE) @ 4.00"± THICK (APPROX. 460.0 LBS./S.Y.) ITEM 307-02.01 ASPHALT CONCRETE MIX (PG70-22)(BPMB-HM) GRADING "A" (TO BE USED FOR BREAKOUT ONLY)
⑦	BITUMINOUS BASE COURSE (BINDER) @ 2.00"± THICK (APPROX. 226.0 LBS./S.Y.) ITEM 307-02.08 ASPHALT CONCRETE MIX (PG70-22)(BPMB-HM) GRADING "B-M2" (TO BE USED FOR PARTIAL-DEPTH REPAIRS ONLY)



BRIDGE NOTES				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE TREATMENT TYPE
36SR0150025	21.18R	Indian Creek	485'-0"	SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED) COLD PLANE 1.5" OF THE EXISTING ASPHALT AND REPLACE WITH 1.5" OF NEW ASPHALT.
36SR0150026	21.18L	Indian Creek	485'-0"	SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED) REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
36SR0150027	22.05R	Panther Branch	22'-0"	PAVE WITH PLANS MIX/TREATMENT TYPE
36SR0150027	22.05L	Panther Branch	22'-0"	PAVE WITH PLANS MIX/TREATMENT TYPE
36SR0150029	22.34R	Flat Gap Creek	244'-0"	SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED) REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
36SR0150030	22.34L	Flat Gap Creek	244'-0"	SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED) REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)

SEALED BY

4/2/2025

NOT TO SCALE

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS AND  
PAVEMENT  
SCHEDULE



GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (11) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.02, SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

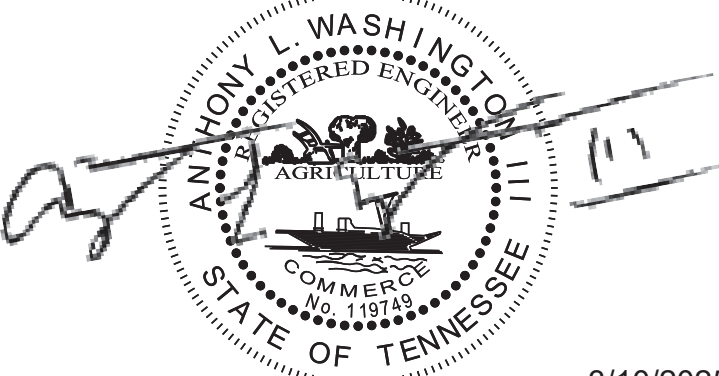
CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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GENERAL  
NOTES



SPECIAL NOTES

RESURFACING

- (1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (2) THE INSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.
- (4) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.
- (5) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-13.02.

SIGNS

- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

- (1) ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.
- (2) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.

JOINT SEALANTS

- (1) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN ALL PAVEMENT LAYERS EXCEPT OGFC SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (2) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (3) TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENTLY TO PREVENT DAMAGE FROM TRACKING.


PAVEMENT

RESURFACING

- (2) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
  - A. BROOMING & DEGRASSING SHALL INCLUDE NOTCHING THE GRAVEL SHOULDER PRIOR TO MILLING. THE NOTCH SHALL BE 1.5 IN DEEP AND 2 FT WIDE, OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.
  - B. ALL MATERIAL FROM NOTCHING AND BROOMING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.
  - C. ALL COSTS ASSOCIATED WITH NOTCHING, PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

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SPECIAL  
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ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4)

THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11)

SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12)

IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15)

IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ENGINEERING PRODUCTION SUPPORT DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4)

ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

SCOPE OF WORK

- (6)

THIS PROJECT INCLUDES COLD PLANING, RESURFACING, PAVEMENT MARKINGS, GUARDRAIL, TEMPORARY TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1)

IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6)

EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31)

CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32)

WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33)

IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35)

WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)

ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37)

ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38)

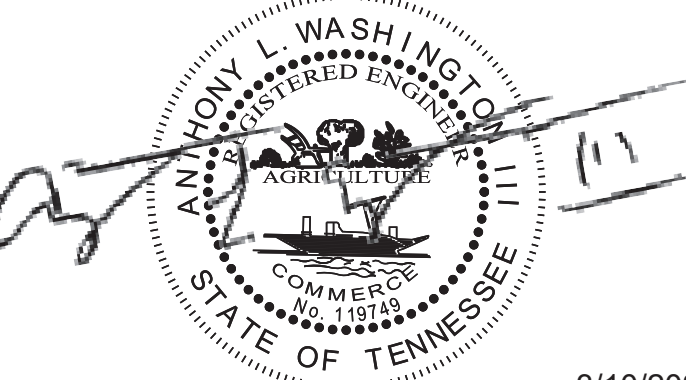
OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39)

DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40)

WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

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ENVIRONMENTAL  
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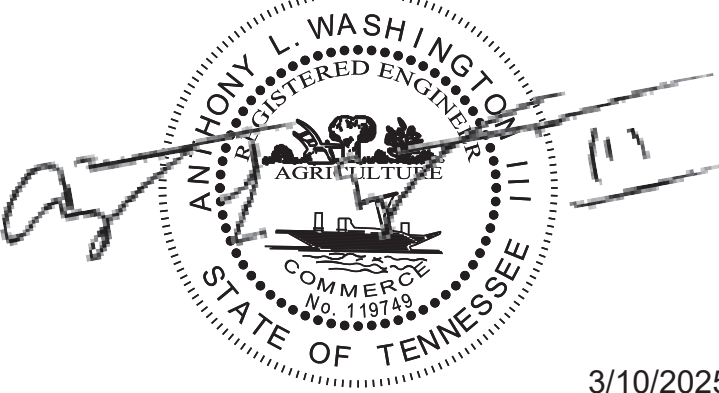
ENVIRONMENTAL NOTES (CONT.)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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ENVIRONMENTAL  
NOTES

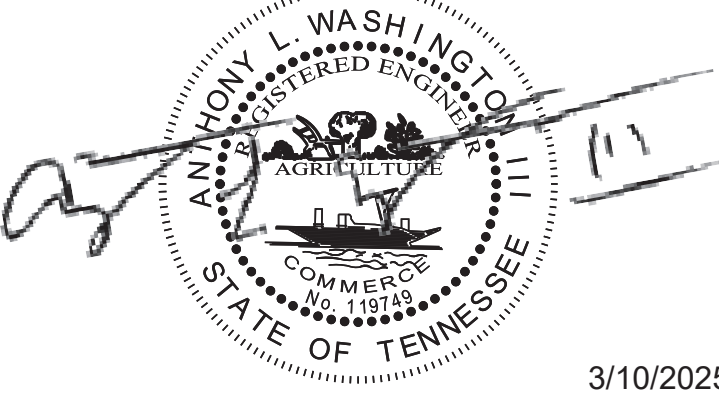


TRAFFIC CONTROL SIGN TABULATION (RESURFACING)							
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES L   x   W		S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.	
G20-1	ROAD WORK NEXT 3 MILES	48"	x	24"	8	4	32
G20-2	END ROAD WORK	48"	x	24"	8	9	72
W8-11	UNEVEN LANES	48"	x	48"	16	13	208
W20-1	ROAD WORK 1 MILE	48"	x	48"	16	4	64
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16	4	64
W20-1	ROAD WORK 1000 FT	48"	x	48"	16	4	64
W20-1	ROAD WORK 500 FT	48"	x	48"	16	4	64
W20-1	ROAD WORK AHEAD	48"	x	48"	16	5	80
W20-4	ONE LANE ROAD 1000 FT - PORTABLE	48"	x	48"	16	2	32
W20-7	FLAGGER (SYMBOL) - PORTABLE	48"	x	48"	16	2	32
W8-15	GROOVED PAVEMENT	48"	x	48"	16	13	208
W8-15P	MOTORCYCLE PLAQUE	30"	x	24"	5	13	65
W21-2	FRESH OIL - PORTABLE	48"	x	48"	16	2	32
						TOTAL	1017

PROPOSED GUARDRAIL (RESURFACING)								
SIDE				LOG MILE	GUARDRAIL		TERMINAL ANCHORS	
					GUARDRAIL TRANSITION 27 IN TO 31 IN	GUARDRAIL REMOVED	TYPE 38	TYPE 13
WB		EB			MASH TL3 (46.875')	MASH TL3 (9.375')		
LT	RT	LT	RT		705-02.10 (EACH)	706-01 (L.F.)	705-06.20 (EACH)	705-06.10 (EACH)
		x			21.29	1	50	1
x				21.31	1	50	1	
			x	22.06	1	10		1
x				22.06	1	10		1
x				22.22	1	10		1
			x	22.41	1	50	1	
x				23.53	1	10		1
x				23.58	1	50	1	
TOTALS					8	240	4	4

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-15(232)	2F

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3/10/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TABULATED  
QUANTITIES



UTILITY NOTES

UTILITY

- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED ‘AROUND’ UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR’S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

**ELECTRIC:**  
**TENNESSEE VALLEY ELECTRIC CO-OP**  
590 FLORENCE ROAD  
SAVANNAH, TN 38372  
CONTACT: GERALD TAYLOR JR.  
OFFICE PHONE: 731 925 4916  
Email: GTAYLORJR@TVEC.COM

**PICKWICK ELECTRIC CO.**  
672 HWY 142  
SELMER, TN 38375  
CONTACT: JOHN HUGHES  
OFFICE PHONE: 731 645 3411  
CELL PHONE: 731 434 0619  
Email: JHUGHES@PICKWICK-ELECTRIC.COM

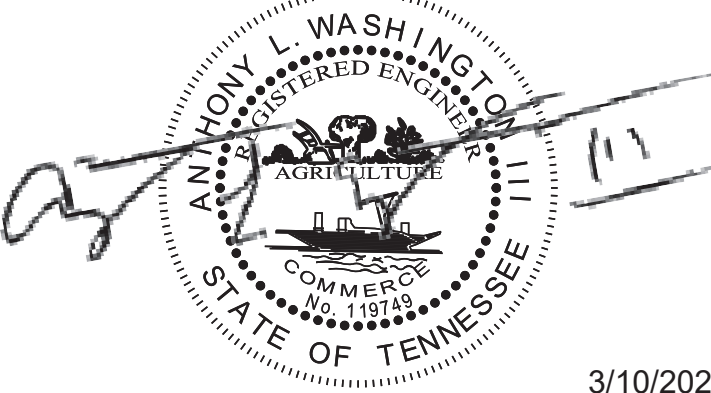
**COMMUNICATIONS:**  
**AT&T**  
315 E. COLLEGE STREET  
JACKSON, TN 38301  
CONTACT: DANIEL R. POTTS  
OFFICE PHONE: 901 488 2359  
Email: DP7607@ATT.COM

**FIBER OPTIC:**  
**CENTURYLINK (QWEST) / LUMEN (LEVEL 3)**  
8110 CORDOVA RD. STE. 101  
CORDOVA, TN 38016  
CONTACT: BRIAN MCGREGOR  
OFFICE PHONE: 901 435 2025  
Email: BRIAN.MCGREGOR@LUMEN.COM

**WATER/GAS/SEWER:**  
**CITY OF SAVANNAH**  
140 MAIN ST.  
SAVANNAH, TN 38372  
CONTACT: GREG LITTLEFIELD  
OFFICE PHONE: 731 925 4216  
Email: GLITTLEFIELD@CITYOFSAVANNAH.ORG

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSSIP-15(232)	3

SEALED BY



3/10/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES  
AND  
UTILITY OWNERS



PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a.

THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d.

THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4.

FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.


b.

ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.
- THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.
- C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:
- SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2.

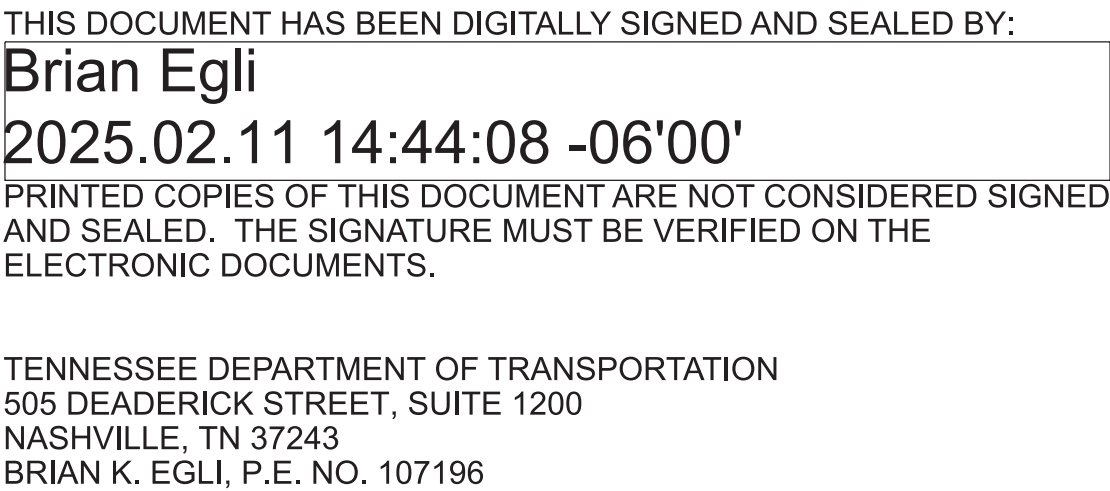
WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.
- |        |      |                 |           |
|--------|------|-----------------|-----------|
| TYPE   | YEAR | PROJECT NO.     | SHEET NO. |
| RESURF | 2025 | NH/HSIP-15(232) | 4         |
|        |      |                 |           |
|        |      |                 |           |
- SEALED BY



3/10/2025
- STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE  
DROP-OFF NOTES  
FOR  
TRAFFIC CONTROL
- 2/19/2025 2:57:09 PM C:\PWORKING\EA\ST01\D3435533\02D.DGN





<b>SHEET NAME</b>	<b>SHEET NO.</b>
SIGNATURE SHEET _____	STRUCTURE-SIGN _____
BRIDGE PLANS _____	B1 THRU B10 _____

YEAR	PROJECT NO.	SHEET NO.
2025	36S015-M3-005	STRUCTURE-SIGN 1
<b>STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION</b>		
<b>SIGNATURE SHEET</b>		

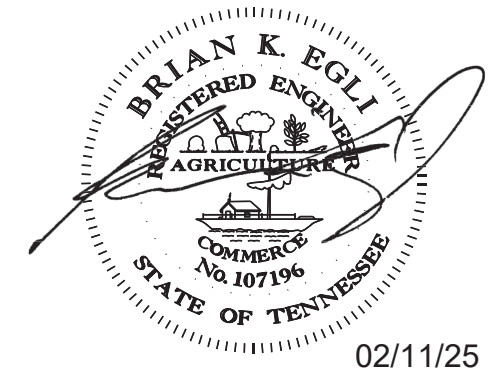


PIN 133162.00

[illegible]

INDEX OF DRAWINGS	DWG. NO.	LAST REV. DATE
SIGNATURE SHEET	STRUCTURE-SIGN 1	
INDEX OF DRAWINGS	B1	
BRIDGE TABULATION, ESTIMATED QUANTITIES, AND EXPANSION JOINT REPAIR NOTES	B2	
TYPE I THIN EPOXY OVERLAY NOTES	B3	
PLAN VIEW (36SR0150025)	B4	
PHASE CONSTRUCTION	B5	
PLAN VIEW (36SR0150026)	B6	
PHASE CONSTRUCTION	B7	
PLAN VIEW (36SR0150029 & 36SR0150030)	B8	
PHASE CONSTRUCTION (36SR0150029)	B9	
PHASE CONSTRUCTION (36SR0150030)	B10	

INDEX OF REFERENCE DRAWINGS	DWG. NO.
LAYOUT OF BRIDGE	M-445-95
SUPERSTRUCTURE	M-445-99
LAYOUT OF BRIDGE	M-445-118
SUPERSTRUCTURE	M-445-122



STATE OF TENNESSEE

## INDEX OF DRAWINGS

HARDIN COUNTY  
2025

B1



PIN 133162.00

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

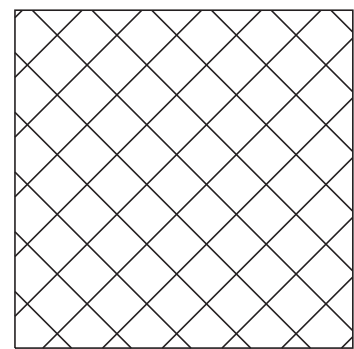
THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.

BRIDGE TABULATION,  
ESTIMATED QUANTITIES, AND  
EXPANSION JOINT REPAIR  
NOTES  
36-SR15-21.18R @ L  
OVER  
INDIAN CREEK  
36-SR15-22.34R @ L  
OVER  
FLAT GAP CREEK  
BR. NOS. 36SR0150025  
36SR0150026  
36SR0150029  
36SR0150030  
HARDIN COUNTY  
2025







[illegible]

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
DRAWN BY Z.HAYNES DATE 8/24  
SUPERVISED BY K. MARTINKO DATE 8/24  
CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_

BRIDGE ID. NO. 36SR0150025

02/11/25



PROJECT NO.	YEAR	SHEET NO.
36S015-M3-005	2025	B5

[illegible]

(LOOKING AHEAD ON SURVEY)



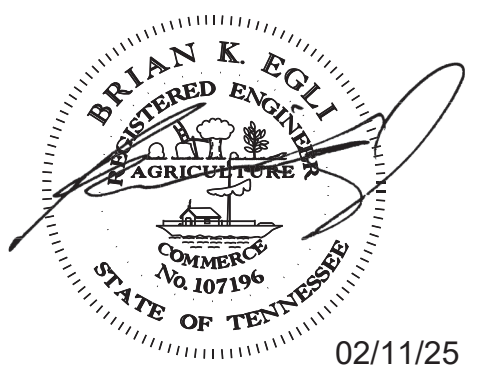
(LOOKING AHEAD ON SURVEY)

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASE CONSTRUCTION  
36-SR15-21.18R  
OVER  
INDIAN CREEK  
BR. NO. 36SR0150025  
HARDIN COUNTY  
2025



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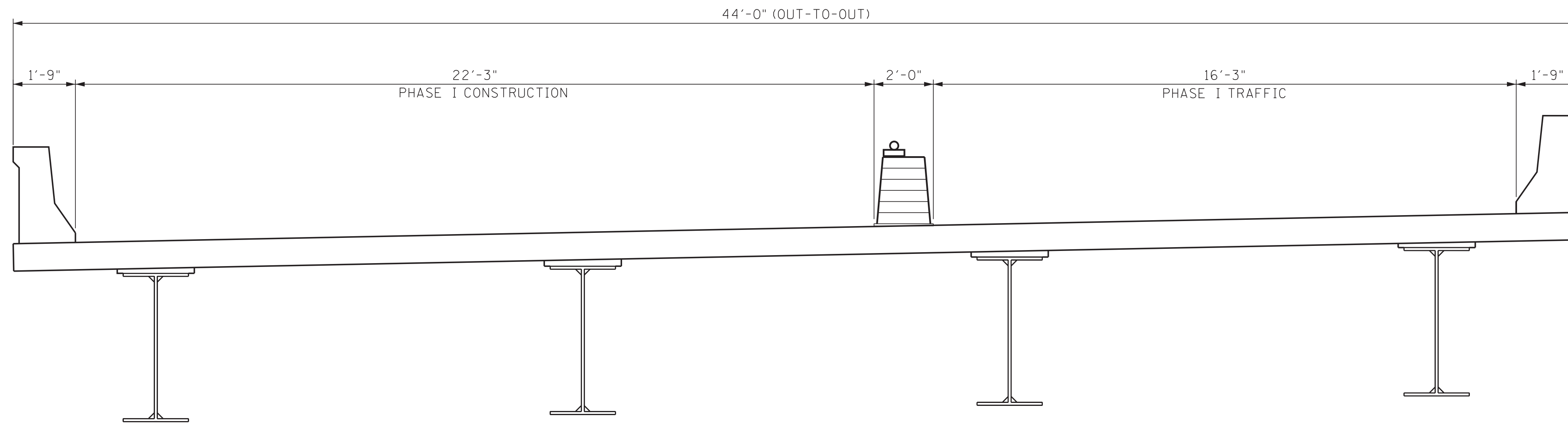
DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
 DRAWN BY Z.HAYNES DATE 8/24  
 SUPERVISED BY K. MARTINKO DATE 8/24  
 CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW  
36-SR15-21.18L  
OVER  
INDIAN CREEK  
BR. NO. 36SR0150026  
HARDIN COUNTY  
2025

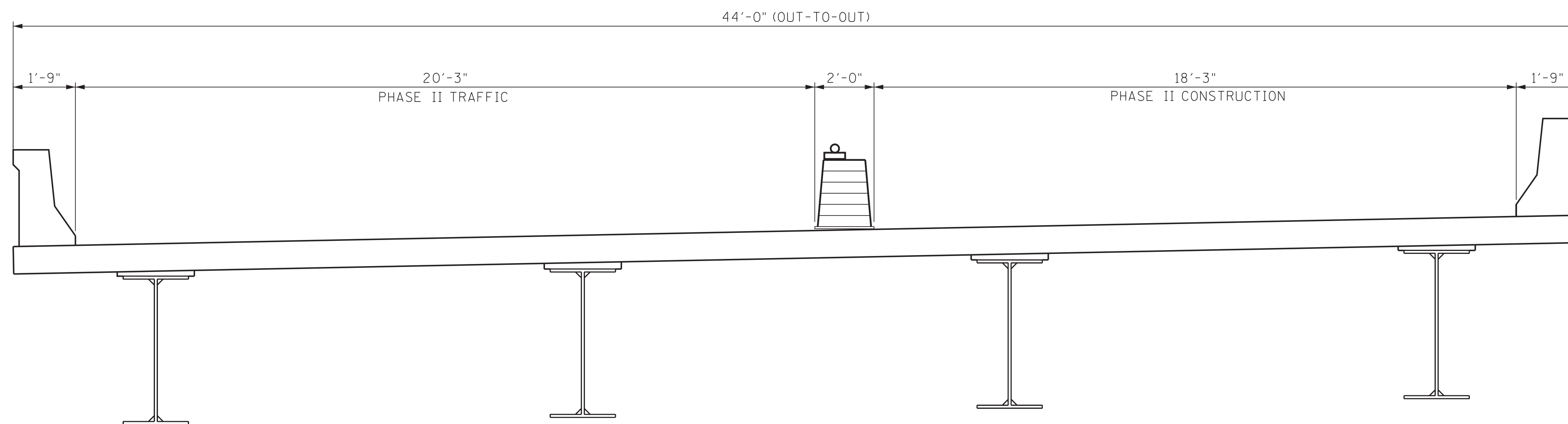


PROJECT NO.	YEAR	SHEET NO.
36S015-M3-005	2025	B7

[illegible]

(36-SR15-21.18L)

(LOOKING AHEAD ON SURVEY)



(36-SR15-21.18L)

(LOOKING AHEAD ON SURVEY)

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
DRAWN BY Z.HAYNES DATE 8/24  
SUPERVISED BY K. MARTINKO DATE 8/24  
CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_

The seal is circular with a dashed outer border. Inside the border, the text "BRIAN K. EGGLEY" is at the top, "REGISTERED ENGINEER" is in the middle, "AGRICULTURE" is below that, and "STATE OF TENNESSEE" is at the bottom. In the center is a graphic of a plow and a sheaf of wheat. Below the graphic, it says "COMMERCIAL" and "No. 107196". A date stamp "02/11/25" is in the bottom right corner of the seal area.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASE CONSTRUCTION  
36-SR15-21.18L  
OVER  
INDIAN CREEK  
BR. NO. 36SR0150026  
HARDIN COUNTY  
2025



[illegible]

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW  
36-SR15-22.34R \$ L  
OVER  
FLAT GAP CREEK  
BR. NOS. 36SR0150029  
36SR0150030  
HARDIN COUNTY  
2025



PROJECT NO.	YEAR	SHEET NO.
36S015-M3-005	2025	B9

[illegible]

## PHASE I CONSTRUCTION

EASTBOUND BRIDGE

## PHASE II CONSTRUCTION

02/11/25

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASE CONSTRUCTION  
36-SR15-22.34R  
OVER  
FLAT GAP CREEK  
BR. NO. 36SR0150029  
HARDIN COUNTY  
2025

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
 DRAWN BY Z.HAYNES DATE 8/24  
 SUPERVISED BY K. MARTINKO DATE 8/24  
 CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_





44'-0" (OUT-TO-OUT)

1'-9"

20'-3"

PHASE II TRAFFIC

2'-0"

18'-3"

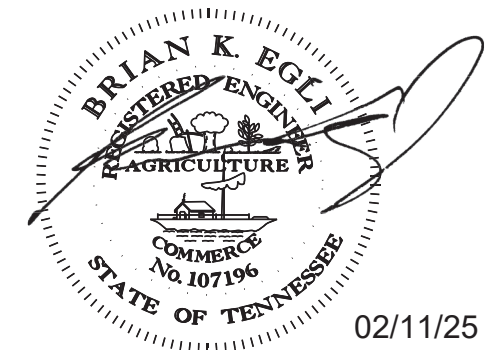
PHASE II CONSTRUCTION

1'-9"

The diagram illustrates a cross-section of a bridge deck with a total width of 44 feet from the outer edge of the curb to the outer edge of the curb. The deck is supported by four vertical I-beam piers. The deck is divided into two main sections: a 20-foot 3-inch section labeled "PHASE II TRAFFIC" and an 18-foot 3-inch section labeled "PHASE II CONSTRUCTION". The traffic section is further divided into a 1-foot 9-inch section on the left and a 2-foot 0-inch section on the right. The construction section is divided into a 2-foot 0-inch section on the left and a 1-foot 9-inch section on the right. A small structure, possibly a vent or a small building, is shown on the right side of the traffic section, within the 2-foot 0-inch section. The diagram also shows the profile of the bridge deck and the piers.

WESTBOUND BRIDGE  
(36-SR15-22.34L)

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
DRAWN BY Z.HAYNES DATE 8/24  
SUPERVISED BY K. MARTINKO DATE 8/24  
CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_

[illegible]

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASE CONSTRUCTION  
36-SR15-22.34L  
OVER  
FLAT GAP CREEK  
BR. NO. 36SR0150030  
HARDIN COUNTY  
2025

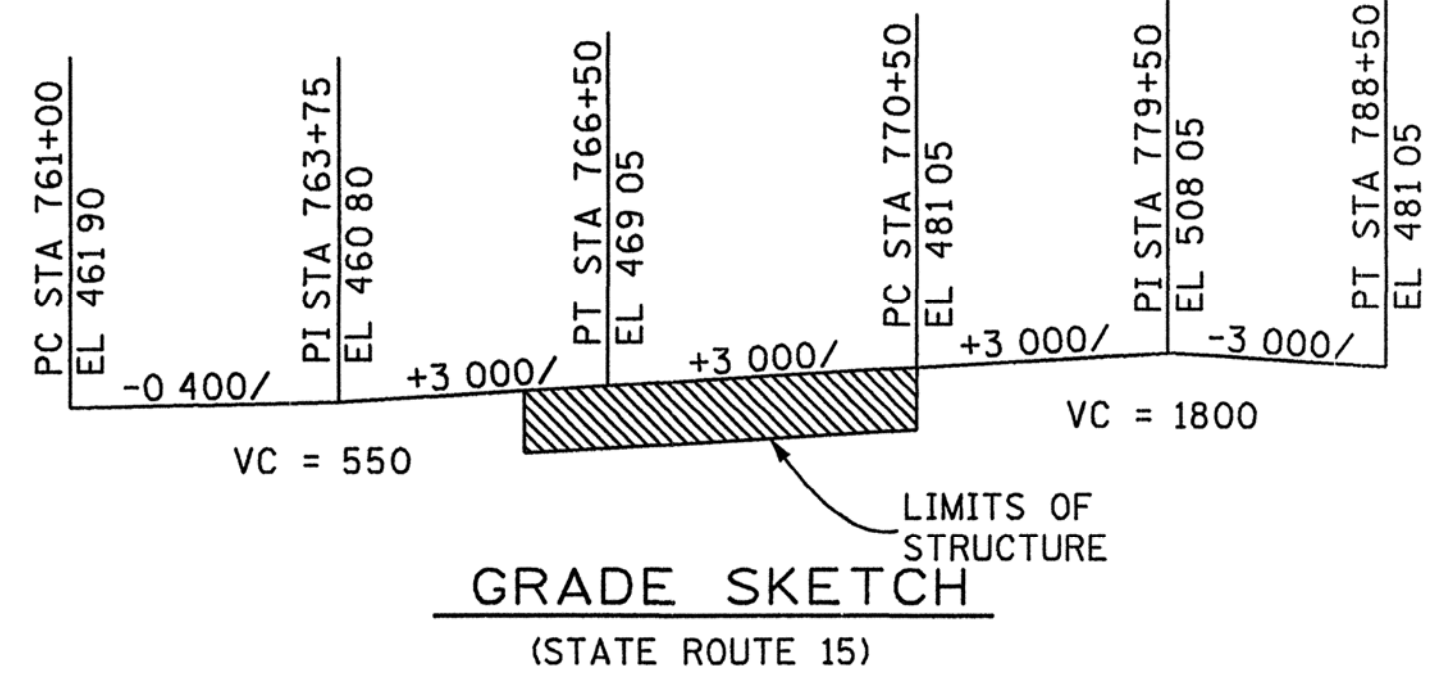
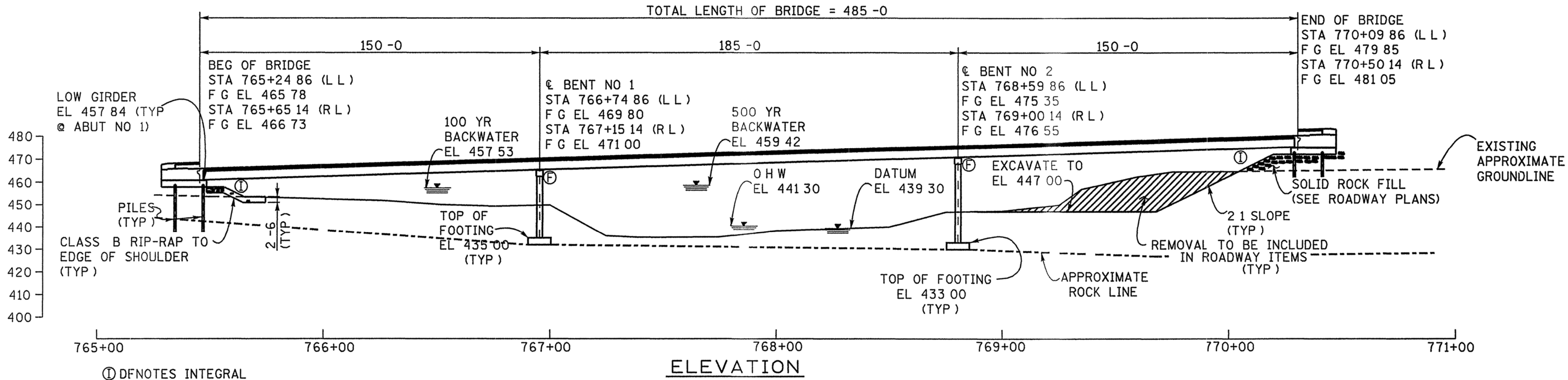


CONST NO 36001-3282-14

PROJECT NO	YEAR	SHEET NO
NHE-15(99)	2003	

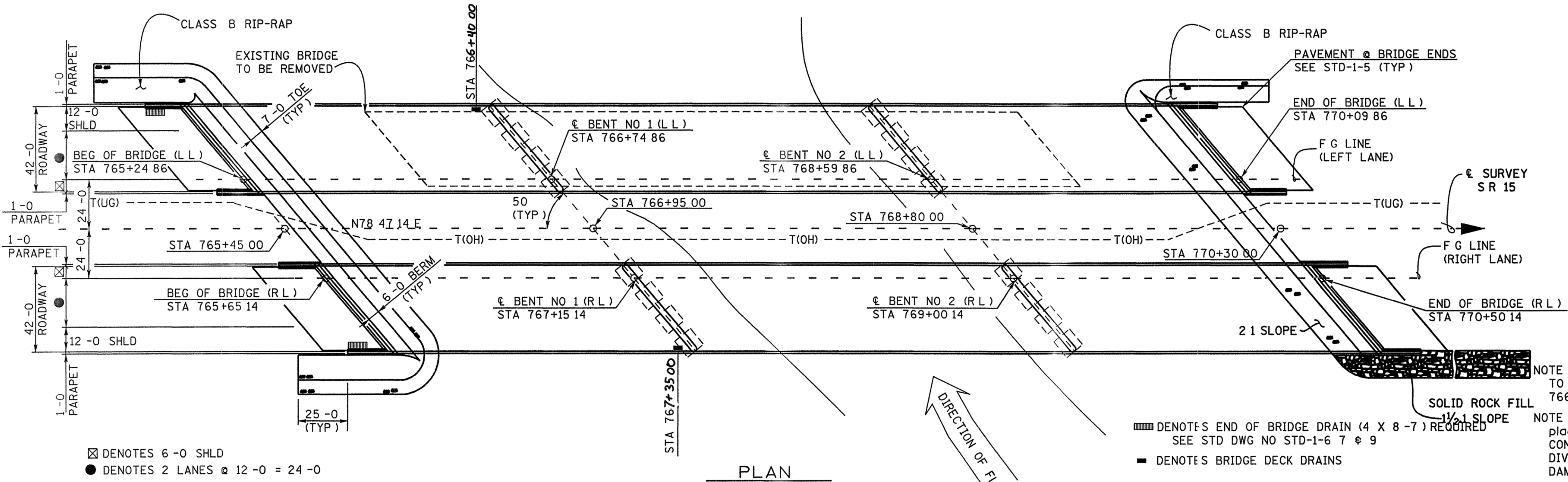
REVISIONS

NO	DATE	BY	BRIEF DESCRIPTION
1	6/9/04	DW	ADDED 4 SHEETS
1	04/14/05	JPC	REVISED LAST REV DATES



HYDRAULIC DATA

DRAINAGE AREA = 160.5 mi<sup>2</sup>  
 DESIGN DISCHARGE 100 YR @ THIS STRUCTURE = 25,901.10 cfs  
 TOTAL 100 YR DISCHARGE = 31,188 cfs  
 FLOW THROUGH BR 36-SR15-20.86 = 5,286.90 cfs  
 WATER AREA PROVIDED BELOW EL 457.37 = 3,586.24 ft<sup>2</sup>  
 100 YR VELOCITY = 7.22 fps  
 100 YR BRIDGE BACKWATER = 1.02 ft  
 ROADWAY OVERTOPPING EL = 461.77  
 100 YR DISCHARGE @ THIS BRIDGE = 25,901.10 cfs @ EL 457.37 ft  
 500 YR DISCHARGE @ THIS BRIDGE = 32,947.69 cfs @ EL 459.29 ft



NOTE: EXISTING STRUCTURE NO 36-SR15-2120 AND APPROACHES TO BE REMOVED TO NATURAL GROUND BETWEEN STATIONS 766+12 AND STATION 769+30.

NOTE: ANY EXCAVATION OF THE STREAM CHANNEL AREA (e.g. for rip-rap or pier placement) SHALL BE SEPARATED FROM FLOWING WATER DURING LOW-FLOW CONDITIONS. THIS SHALL BE ACCOMPLISHED BY THE USE OF FLUMES LINED DIVERSION CHANNEL WITH SAND BAG BERM DIVERSION PIPE WITH SAND BAG DAM AT PIPE INLET OR IN SOME CASES COFFERDAMS.

LIST OF DRAWINGS

- LAYOUT OF BRIDGE
- GENERAL NOTES
- ESTIMATED QUANTITIES
- FOUNDATION DATA
- SUPERSTRUCTURE
- SUPERSTRUCTURE DETAILS
- STRUCTURAL STEEL DETAILS
- STRUCTURAL STEEL DETAILS
- STRUCTURAL STEEL DETAILS
- ABUTMENT NO 1 (LEFT LANE)
- ABUTMENT NO 2 (LEFT LANE)
- ABUTMENT NO 1 (RIGHT LANE)
- ABUTMENT NO 2 (RIGHT LANE)
- ABUTMENT NO 1 & 2 LEFT & RIGHT LANES DETAIL
- BENT NO 1 LEFT LANE
- BENT NO 2 LEFT LANE
- BENT NO 2 LEFT LANE
- BENT NO 1 RIGHT LANE
- BENT NO 2 RIGHT LANE
- BENT NO 2 RIGHT LANE
- BENT NO 1 & 2 RIGHT & LEFT LANES
- BENT NO 1 & 2 RIGHT & LEFT LANES
- FINAL FOUNDATION DATA (LEFT LANE)
- FINAL FOUNDATION DATA (RIGHT LANE)
- BILL OF STEEL LEFT LANE
- BILL OF STEEL RIGHT LANE
- BILL OF STEEL RIGHT LANE

DWG NO	LAST REV DATE
M-445-95	04-14-05
M-445-96	
M-445-97	
M-445-98	
M-445-99	
M-445-100	
M-445-101	
M-445-102	06-09-04
M-445-103	06-09-04
M-445-104	
M-445-105	
M-445-106	
M-445-107	06-09-04
M-445-108	06-09-04
M-445-109	
M-445-110	
M-445-110A	06-09-04
M-445-111	
M-445-112	
M-445-112A	06-09-04
M-445-113	
M-445-113A	06-09-04
M-445-114	
M-445-115	
M-445-116	
M-445-117	
M-445-117A	06-09-04

LIST OF STANDARD DRAWINGS

- BRIDGE RAILING CONCRETE PARAPET
- SLIDER PLATES AND DECK DRAINS
- PAVEMENT AT BRIDGE ENDS
- BRIDGE END DRAIN W/PABE
- BRIDGE END DRAIN W/PABE
- BRIDGE END DRAIN 4 X 8-7 W/PABE
- STD PRECAST PRESTRESSED BRIDGE DECK PANELS
- GENERAL DETAILS
- STD PRECAST PRESTRESSED BRIDGE DECK PANELS
- DESIGN CRITERIA
- STD PRECAST PRESTRESSED BRIDGE DECK PANELS
- GENERAL DETAILS
- STD PRECAST PRESTRESSED BRIDGE DECK PANELS
- CONSTRUCTION DETAILS
- STANDARD PILE DETAILS
- STANDARD PILE DETAILS
- REINF BAR SUPPORT DETAILS FOR CONC SLABS
- MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS

DWG NO	LAST REV. DATE
STD-1-1	07-31-00
STD-1-2	09-11-95
STD-1-5	07-31-00
STD-1-6	04-28-97
STD-1-7	07-31-00
STD-1-9	05-01-95
STD-4-1	05-21-99
STD-4-2	06-10-96
STD-4-3	03-02-02
STD-4-4	06-10-96
STD-5-1	10-25-93
STD-5-2	05-21-99
STD-9-1	12-19-94
STD-10-1	05-11-92

LIST OF SPECIAL PROVISIONS

- REGARDING APPROVAL OF SHOP DRAWINGS
- REGARDING STEEL STRUCTURES

PROV NO	LAST REV. DATE
105A	12-15-97
602	3-18-96



2022 ADT = 5,200  
 2 @ 42 ROADWAY w/STD-1-1 BRIDGERAIL  
 DESIGN SPEED = 65 MPH

BRIDGE NO. 1

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

LAYOUT  
 STATE ROUTE 15  
 OVER INDIAN CREEK  
 BRIDGE I.D. NO. 36SR0150025 (R.L.)  
 BRIDGE I.D. NO. 36SR0150026 (L.L.)  
 L.M. 21.20  
 STATION 767+82.50  
 HARDIN COUNTY  
 2003

CORRECT *Edward P. Wasserman*  
 ENGINEER OF STRUCTURES



NOTE: THE CONCRETE DECK SHALL NOT BE POURED UNTIL ALL STRUCTURAL STEEL IS ERECTED AND ALL WELDING AND/OR BOLTING COMPLETE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR BRIDGERAIL. THE BRIDGERAIL SHALL NOT BE POURED UNTIL THE SLAB SECTION IS POURED AND CURED.

APPROVAL OF MATERIALS: NO FABRICATION SHALL BE STARTED UNTIL THE MATERIALS INVOLVED HAVE BEEN APPROVED BY THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS.

IDENTITY OF MAIN MATERIALS: SEE SECTION 602 OF THE STANDARD SPECIFICATIONS.

STRUCTURAL STEEL: SHALL CONFORM TO AASHTO M270 (ASTM A709) GRADE 50W UNLESS OTHERWISE NOTED. ALL STRUCTURAL STEEL FOR GIRDER FLANGES IN TENSION AND ALL WEBS SHALL MEET THE SUPPLEMENTAL REQUIREMENTS FOR LONGITUDINAL CHARPY V-NOTCH TESTS SPECIFIED IN AASHTO MATERIAL SPECIFICATIONS, ZONE 2 OF NON-FRACTURE CRITICAL CRITERIA SHALL APPLY.

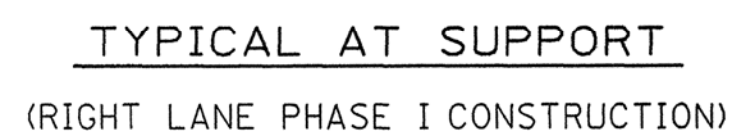
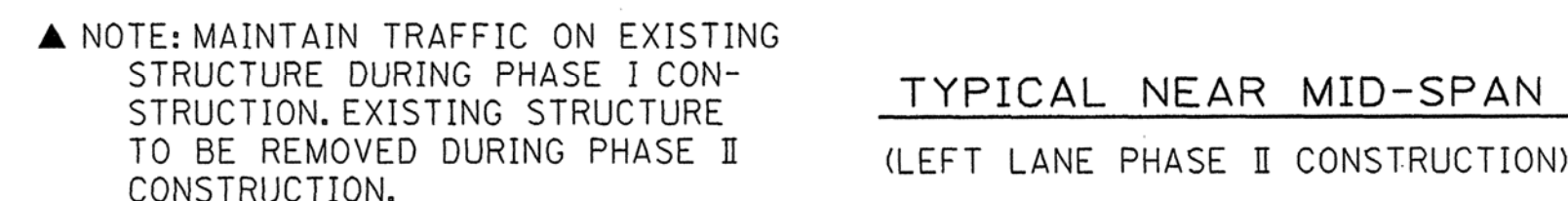
WELDING: ANSI/AASHTO/AWS D1.5-2002 BRIDGE WELDING CODE AND SECTION 602 OF THE STANDARD SPECIFICATIONS.

FIELD CONNECTIONS: SHALL BE  $\frac{7}{8}$ "  $\emptyset$  HIGH TENSILE STRENGTH BOLTS ASTM-A325 TYPE 3 UNLESS OTHERWISE SHOWN. SEE AASHTO SPECIFICATIONS ART.11.3.2 DIVISION II AND SECTION 602 OF THE STANDARD SPECIFICATIONS.

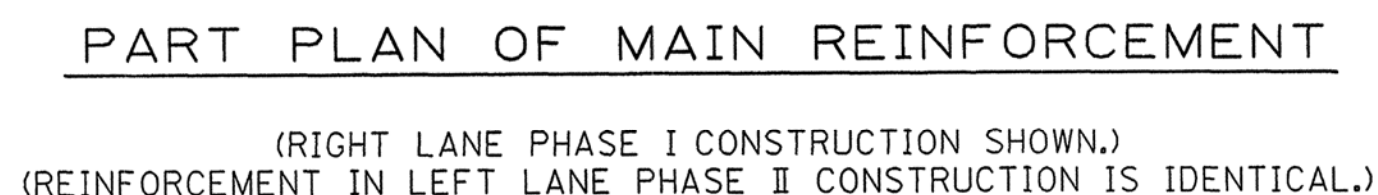
ADDITIONAL SHOP SPlice NOTE: SHOP SPICES NECESSARY DUE TO LENGTHS OR SIZE OF MATERIAL INVOLVED MAY BE ADDED BY THE FABRICATOR SUBJECT TO APPROVAL BY THE ENGINEER AND SHALL BE AT NO ADDITIONAL COST TO THE PROJECT.

ADDITIONAL FIELD SPlice NOTE: FIELD SPICES NECESSARY DUE TO LENGTHS INVOLVED MAY BE ADDED BY THE FABRICATOR SUBJECT TO APPROVAL BY THE ENGINEER AND SHALL BE AT NO ADDITIONAL COST TO THE PROJECT.

SHOP ASSEMBLY: PROGRESSIVE SHOP ASSEMBLY WILL BE ALLOWED. SEE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, ART.11.5.3.1 DIVISION II.



▽ DENOTES 1'-4" (TYP. @ CANTILEVER)  
 ☆ DENOTES 1'-9"  
 ● DENOTES 3 SPA. @ 9" = 2'-3"  
 ● DENOTES BARS A501E  $\frac{5}{8}$ " BARS A502E  
 (TYP. @ CANTILEVER)  
 ☒ DENOTES 5 SPA. @ 4" = 1'-8"  
 (TYP. @ CANTILEVER)



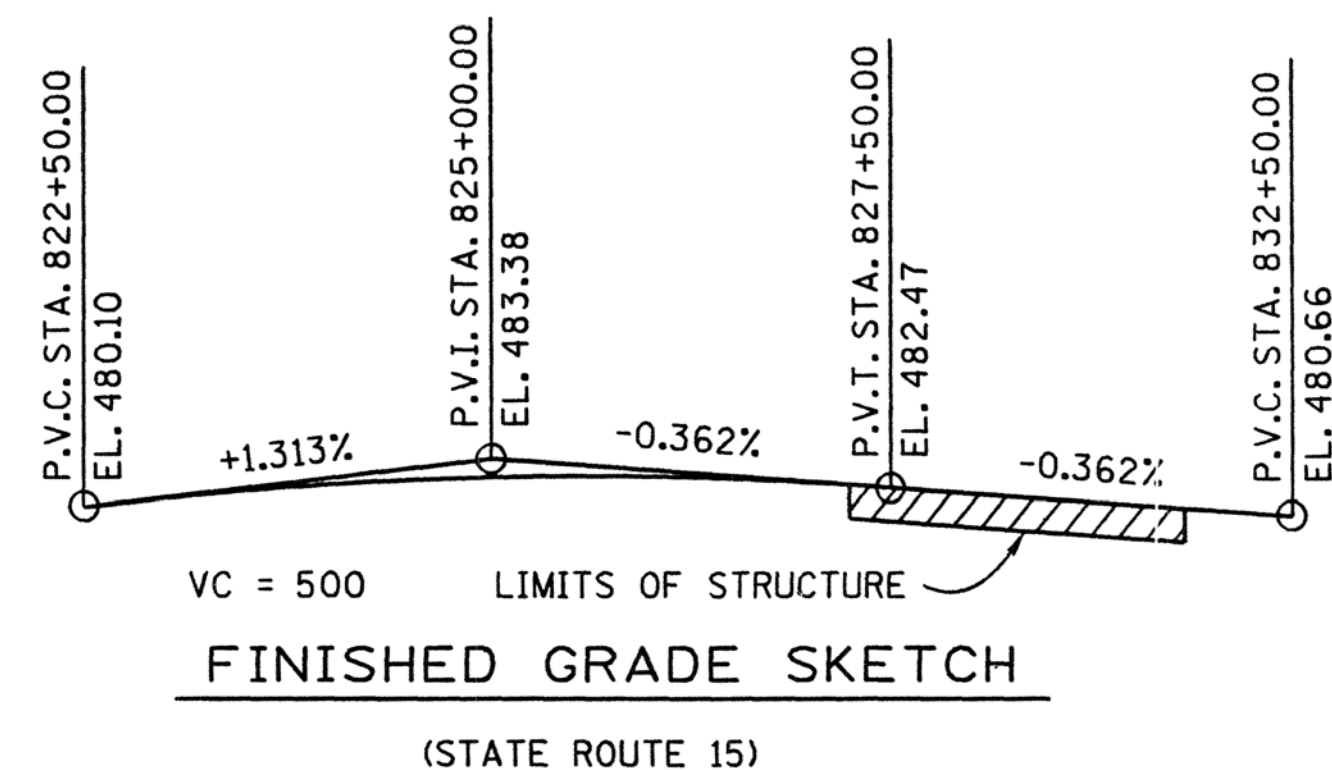
	CLASS 'D' CONCRETE (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.
RIGHT LANE	649	155,012
LEFT LANE	949	155,012

BRIDGE NO. 1  
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
SUPERSTRUCTURE  
STATE ROUTE 15  
OVER  
INDIAN CREEK  
STATION 767+82.50  
LOG MILE 21.20  
HARDIN COUNTY  
2003



CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES





LIST OF STANDARD DRAWINGS	DWG. NO.	LAST REV. DATE
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LIST OF SPECIAL PROVISIONS	PROV. NO.	LAST REV. DATE
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HYDRAULIC DATA

DRAINAGE AREA = 6.53 MI<sup>2</sup>  
DESIGN DISCHARGE (100 YR.) = 3424 CFS  
WATER AREA PROVIDED BELOW EL. 476.73 = 873.68 FT.<sup>2</sup>  
100 YR. VELOCITY = 3.92 FPS  
100 YR. BRIDGE BACKWATER = 0.55 FT. @ EL. 476.73 FT.  
100 YR. DISCHARGE = 3424 CFS @ EL. 476.73 FT.  
500 YR. DISCHARGE = 4610 CFS @ EL. 478.51 FT.  
ROADWAY OVERTOPPING EL. = 479.69 FT.

2 @ 42'-0" ROADWAY WITH STD-1-1 PARAPET  
2022 ADT = 5200  
DESIGN SPEED = 65 MPH

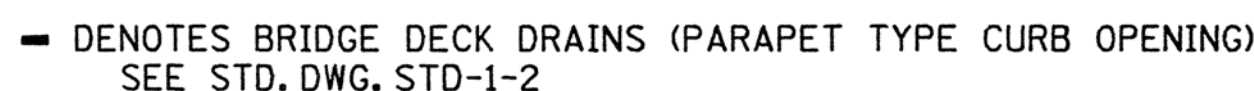
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION


BRIDGE NO. 2  
LAYOUT OF BRIDGE  
STATE ROUTE 15  
OVER  
FLAT GAP CREEK  
STATION 828+35.00  
LOG MILE 22.34  
HARDIN COUNTY  
2003




CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES

M-445-118



 DENOTES 2' x 8'-7" END OF BRIDGE DRAINS.  
 SEE STD-1-6.7 & 8.


 DENOTES EXISTING BRIDGE (NO. 36-SR15-22.30) AND APPROACHES TO BE REMOVED DURING PHASE II CONSTRUCTION TO NATURAL GROUND BETWEEN STA. 826+00.00 (NW CORNER) AND STA. 827+49.71 (NE CORNER). STRUCTURE IS 149'-8½" IN LENGTH AND 34'-9" WIDE. IT CONSISTS OF 5 SPANS OF CONCRETE SLAB WITH CURBS AND TIMBER PIER CAPS AND PILES.

--T(UG)-- DENOTES TELEPHONE LINE.

NOTE: ANY EXCAVATION OF THE STREAM CHANNEL AREA (E.G. FOR RIP-RAP PLACEMENT) SHALL BE ACCOMPLISHED IN THE DRY DURING LOW-FLOW CONDITIONS. THIS SHALL BE ACCOMPLISHED BY THE USE OF COFFERDAMS, FLUMES, LINED DIVERSION CHANNEL WITH SAND BAG BERM, OR DIVERSION PIPE WITH SAND BAG DAM AT PIPE INLET.

## PLAN

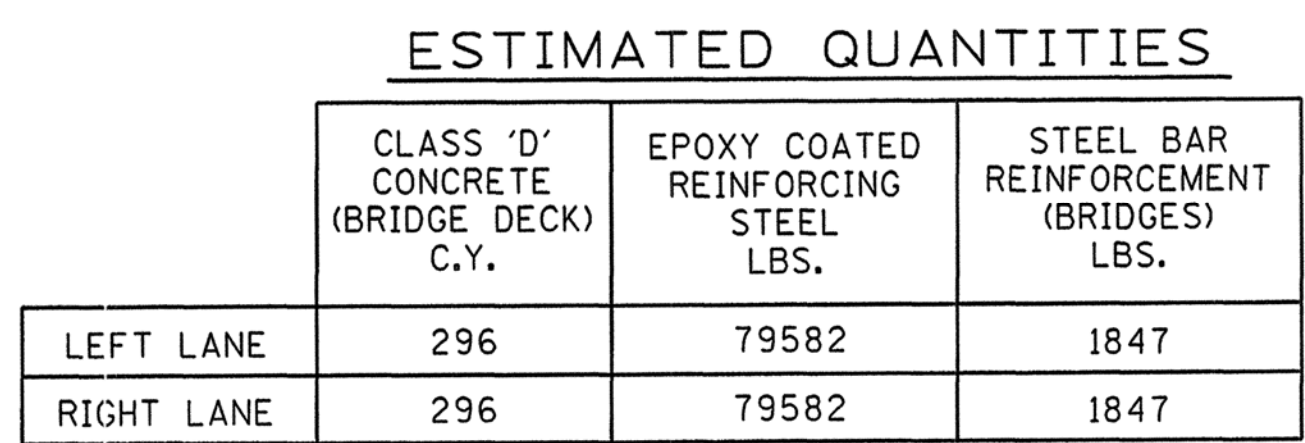
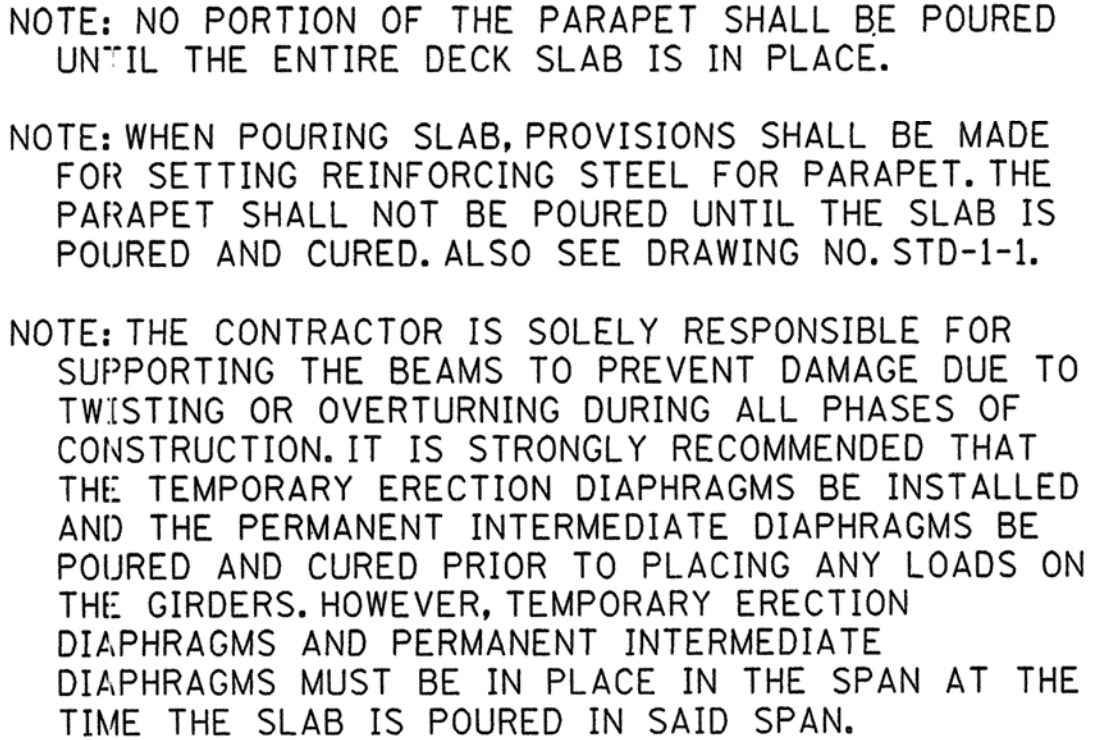
SCALE: 1" = 30'

## ① LIST OF DRAWINGS

LAYOUT OF BRIDGE	M-445-118	06-09-04
GENERAL NOTES	M-445-119	
ESTIMATED QUANTITIES	M-445-120	
FOUNDATION DATA	M-445-121	
SUPERSTRUCTURE	M-445-122	
SUPERSTRUCTURE DETAILS	M-445-123	
PRESTRESSED I-BEAM DETAILS	M-445-124	
ABUTMENT NO.1 (LL) AND ABUTMENT NO.2 (RL)	M-445-125	
ABUTMENT NO.1 (RL) AND ABUTMENT NO.2 (LL)	M-445-126	
ABUTMENT NO.1 AND 2 DETAILS (LL & RL)	M-445-127	
BENT NO.1 AND 2 (LL)	M-445-128	
BENTS NO.1 AND 2 (LL)	M-445-128A	06-09-04
BENT NO.1 AND 2 (RL)	M-445-129	
BENT NO.1 AND 2 DETAILS (LL & RL)	M-445-129A	
BENTS NO.1 AND 2 (RL)	M-445-129B	06-09-04
BENT NO.1 AND 2 DETAILS	M-445-129C	06-09-04
FINAL FOUNDATION DATA (LL)	M-445-130	
FINAL FOUNDATION DATA (RL)	M-445-131	
BILL OF STEEL	M-445-132	
BILL OF STEEL	M-445-133	
GENERAL NOTES & BILL OF STEEL	M-445-133A	06-09-04

DESIGNED BY J. MATELSKI (CW) DATE 01-01  
 DRAWN BY DIANE BUSH DATE 01-03  
 SUPERVISED BY RLH / (KRE) DATE 01-01  
 CHECKED BY SMS DATE 01-03





BRIDGE NO. 2  
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
SUPERSTRUCTURE  
STATE ROUTE 15  
OVER  
FLAT GAP CREEK  
STATION 828+35.00  
LOG MILE 22.34  
HARDIN COUNTY  
2003



CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES